
CITY OF KELOWNA

MEMORANDUM

DATE: January 4, 2008

TO: City Manager

FROM: Planning and Development Services Department

APPLICATION NO. OCP07-0028
Z07-0085

OWNER: Susan Fairweather
Kim Kabella
Afshin Khodarahmi
Allaire Properties (Lakeshore) INC.NO.BC0803827
Jeffrey & Bonnie Devitt
Erzsebet & Stephen Kabella

AT: 4429, 4433 & 4439 Lakeshore Road
586, 588 & 606 Sherwood Road

APPLICANT: Allaire Properties (Lakeshore) Inc.
(Marc Allaire)

PURPOSE: TO AMEND THE OFFICIAL COMMUNITY PLAN FUTURE LAND USE DESIGNATION FROM SINGLE/TWO FAMILY TO LOW DENSITY MULTIPLE UNIT RESIDENTIAL AND TO REZONE THE SUBJECT PROPERTIES FROM RU1 – LARGE LOT HOUSING TO RM3 – LOW DENSITY MULTIPLE HOUSING. THE APPLICANT INTENDS TO CONSTRUCT A 65 UNIT RESIDENTIAL TOWNHOME DEVELOPMENT.

EXISTING OCP DESIGNATION: SINGLE/TWO UNIT RESIDENTIAL
EXISTING ZONE: RU1 – LARGE LOT HOUSING

PROPOSED OCP DESIGNATION: MULTIPLE UNIT RESIDENTIAL – LOW DENSITY
PROPOSED ZONE: RM3 – LOW DENSITY MULTIPLE HOUSING

REPORT PREPARED BY: Alec Warrender

1.0 RECOMMENDATION

THAT OCP Bylaw Amendment No. OCP07-0028 to amend Map 19.1 of the *Kelowna 2020* - Official Community Plan Bylaw No. 7600 by changing the Future Land Use designation of Lot 1, District Lot 358, ODYD, Plan 7524, located at 4429 Lakeshore Road, Lot 4, District Lot 167, ODYD, Plan 1691, located at 4439 Lakeshore Road, Lot 3, District Lot 358, ODYD, Plan 25489, located at 4433 Lakeshore Road, Lot 1, District Lot 167, ODYD, Plan 32648, located at 586 Sherwood Road, Lot 2, District Lot 167, ODYD, Plan 32648, located at 588 Sherwood Road, Lot B, District Lot 167 and 358, ODYD, Plan 32308, located at 606 Sherwood Road, Kelowna B.C., from the Single/Two Unit Residential designation to the Multiple Unit Residential - Low Density designation, as shown on Map "A" attached to the report of Planning & Development Services Department, dated January 4, 2008, be considered by Council;

AND THAT Council considers the APC public process to be appropriate consultation for the purpose of Section 879 of the *Local Government Act*, as outlined in the report of the Planning & Development Services Department dated January 4, 2008;

THAT Rezoning Application No. Z07-0085 to amend the City of Kelowna Zoning Bylaw No. 8000 by changing the zoning classification of Lot 1, District Lot 358, ODYD, Plan 7524, located at 4429 Lakeshore Road, Lot 4, District Lot 167, ODYD, Plan 1691, located at 4439 Lakeshore Road, Lot 3, District Lot 358, ODYD, Plan 25489, located at 4433 Lakeshore Road, Lot 1, District Lot 167, ODYD, Plan 32648, located at 586 Sherwood Road, Lot 2, District Lot 167, ODYD, Plan 32648, located at 588 Sherwood Road, Lot B, District Lot 167 and 358, ODYD, Plan 32308, located at 606 Sherwood Road, Kelowna B.C., from the RU1 – Large Lot Housing zone to the RM3 – Low Density Multiple Housing zone be considered by Council; -

THAT the OCP Bylaw Amendment No. OCP07-0028 bylaw and the zone amending bylaw be forwarded to a Public Hearing for further consideration

AND THAT final adoption of the zone amending bylaw be considered subsequent to the registration of a plan of subdivision to consolidate properties into one title;

AND THAT final adoption of the zone amending bylaw be considered in conjunction with Council's consideration of a Development Permit on the subject properties;

AND THAT final adoption of the zone amending bylaw be considered in conjunction with Council's consideration of a Development Variance Permit on the subject properties;

AND THAT final adoption of the zone amending bylaw be considered subsequent to the requirements of the Inspection Services Department and Works & Utilities Department being completed to their satisfaction;

2.0 SUMMARY

The applicant is requesting Council approval to amend the Official Community Plan's Future Land Use Designation for the subject properties from Single/Two Unit Residential to Multiple Unit Residential – Low Density and to rezone the subject properties from RU1 - Large Lot Housing to RM3 - Low Density Multiple Housing. The applicant has also concurrently applied for a Development Permit for the construction of a 65 unit residential townhouse development. A Development Variance Permit is also being applied for in order to vary the projecting front entry stairs from 0.6m permitted to 1.0m proposed for buildings 1,3,4,6 & 8.

3.0 ADVISORY PLANNING COMMISSION

At a meeting held on November 6, 2007, the APC passed the following motions:

MOVED to Table the Proposal pending additional information from the applicant regarding the affordable housing component.

At the meeting on November 20, 2007, the APC reviewed the application as unfinished business. As the City's affordable housing Bylaws are very weak at the moment and need to be strengthened, the APC deferred the application and passed the following motion:

THAT Council recognizes the affordable housing component is an issue to the entire community and rules/policy need to be in place to enforce this matter for all developers.

4.0 THE PROPOSAL

The main access onto the site will be via Sherwood Road. The required parking will be located at grade in private garages beneath the living areas of the proposed development. The required

visitor parking stalls are spread throughout the site. The proposed development comprises of thirteen buildings containing a total of 65 units. Five of the proposed buildings contain six units, one building has five units and six of the buildings have four units.

The units located on the exterior edges of the development will have direct pedestrian access to Lakeshore and/or Sherwood Roads. The units located internally will have access and patio space fronting either the central common space or the rear/side yard. All of the units have internal vehicle access and doors from the lower floor onto the internal portion of the development. While the units vary in size, each has the living area on the main floor and three bedrooms on the second floor. All of the units have balconies.

The proposed Floor Area Ratio (FAR) takes advantage of the City's density bonus system. Normally a FAR of .50 is permitted. However, when all the parking is located beneath the development an additional 0.2 is permitted. As such, this project is permitted an FAR of 0.70. The applicant is proposing an FAR of .70 and a sight coverage of 39% (Including driveways).

A significant amount of landscaping is proposed. The landscaping along the outer edges of the development will provide a good buffer adjacent to the existing single family residential neighbourhood, while the internal landscaping is also of high quality. A few of the existing trees will be retained in strategic locations. The centre of the development will have a large common area with a walkway and gathering space that is well landscaped and includes a naturalized water feature comprising of two reflecting ponds linked by a small stream.

The applicant has used a mix of building materials and colours to provide texture to the development. The lower portion of the external wall along the properties line features ledgerstone. The wood trim is white and the Hardie siding is wood textured and beige, the Hardie shakes are light beige. The peaks along the roofline and the window boxes provide the development with a residential character. Further, with direct pedestrian access to Sherwood and Lakeshore Roads results in a development that is well connected to the neighbourhood.

Variances

The applicant is proposing a variance. This variance is the result of the applicant's desire to create a common area at the heart of the development. The stairs encroaching into the front and side yards result from shifting the buildings out in an attempt to create more space within the development. Although the private open space requirements are met, the private open space for each unit has been reduced in an attempt to create a larger common area. (See – Variance Justification Letter).

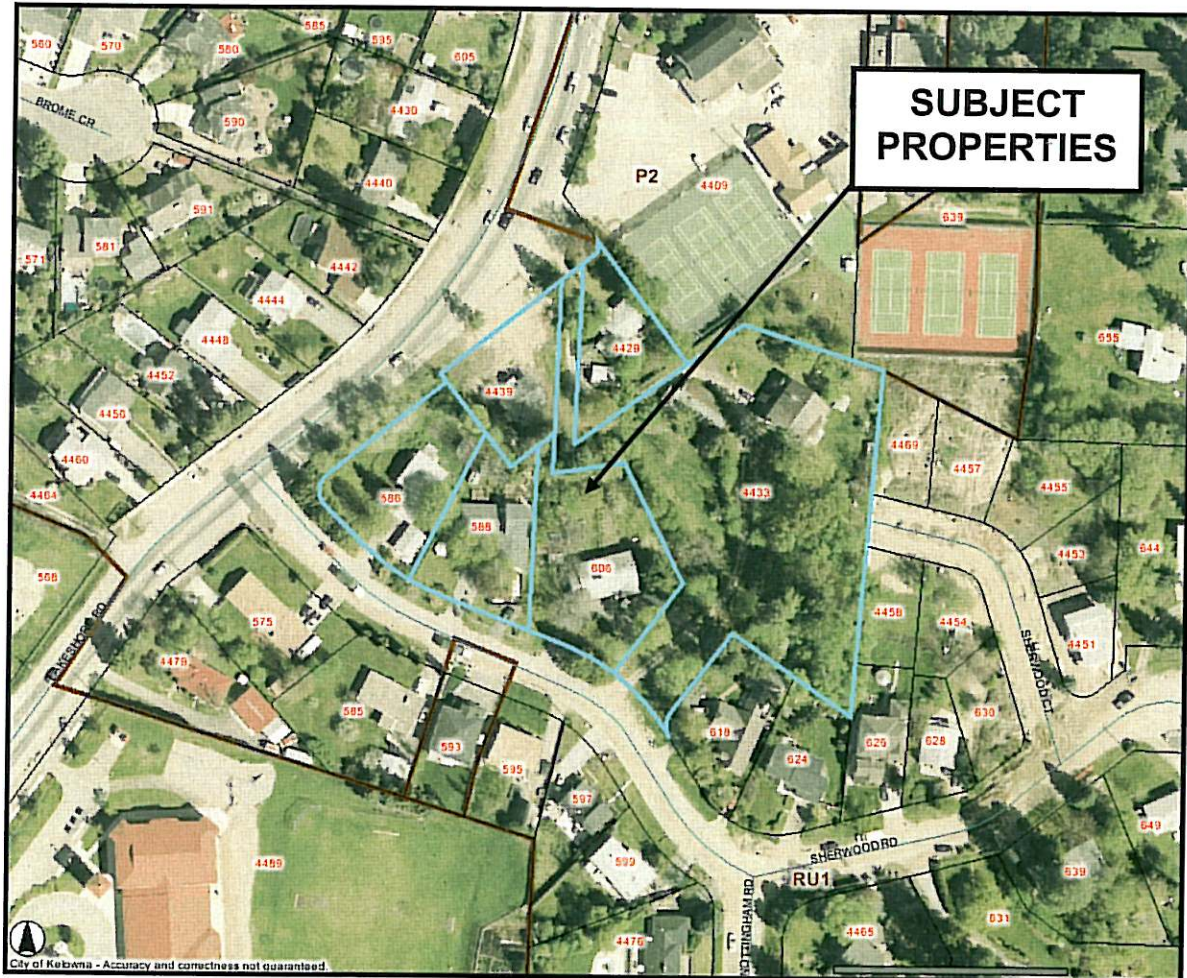
4.1 Site Context

The subject properties are located on the NE corner of Lakeshore and Sherwood Roads. Adjacent zones and land uses are:

<i>Direction</i>	<i>Zoning Designation</i>	<i>Land Use</i>
North	P2 – Education & Minor Institutional	Okanagan Mission Community Hall
East	RU1 – Large Lot Housing	Residential
South	RU1 – Large Lot Housing	Residential
West	RU1 – Large Lot Housing	Residential

4.2 Site Location Map

Corner of Lakeshore & Sherwood Roads



The application conforms to the City's Zoning Bylaw No. 8000 as follows:

CRITERIA	PROPOSAL	RM3 REQUIREMENTS
Site Area (m ²)	13,255m ²	900 m ²
Site Width (m)	91.8 m	30.0 m
Site Depth (m)	133 m	30.0 m
Development Regulations		
Floor Area Ratio (FAR)	0.7	0.5 (0.55 with a housing agreement + 0.2 for underground parking) = 0.75
Height (m)	8.3 m	9.5 m
Storeys (#)	2.5 storeys	2.5 storeys
Setbacks (m)		
- Front (west)	4.5 m (V')	4.5 m

- Rear (east)	7.5 m	7.5 m
- Side (north)	4.5 m	4.5 m
- Side (south)	4.5 m	4.5 m
Private open space	18.8 m ² per unit (Including the central public space the open space requirements are met)	25m ²
Separation between principal buildings	3.0m	3.0m
Parking Stalls (#)	130 spaces 10 visitor spaces	130 spaces (2 per 3 bedroom) 10 Visitor spaces

(V¹) Variance is required for projecting front entry stairs from 0.6m required to 1.0m proposed.

5.0 CURRENT DEVELOPMENT POLICY

Staff recommends that the APC public process should be considered appropriate consultation for the purpose of Section 879 of the *Local Government Act*, and that the process is sufficiently early and does not need to be further ongoing in this case. Furthermore, additional consultation with the Regional District of Central Okanagan is not required in this case.

Staff have reviewed this application, in regards to the City's financial plan and waste management plan and the project may proceed. However, if this project does go forward it may have an impact on the financing and servicing plan. Servicing in this area is nearing capacity. If this project is approved, this area may not be able to accommodate much more development in the future. Staff will be reluctant to support any further OCP amendments in the North Mission / Crawford Area.

OCP07-0018

The subject properties are designated as Single / Two Unit Residential in the OCP as such the proposed use of this site would not be in compliance. However, the OCP supports the sensitive infill of increased density in all areas of the City, and furthermore provides for the potential to increase the density if is an affordable housing component. The applicant has proposed to contribute cash-in-lieu of a Housing Agreement. As such, the total money the applicant has proposed to contribute to the City's Affordable Housing Reserve Fund is \$100,000.

5.1 Current Development Policy

5.1.1 Kelowna Official Community Plan

The future land used designation for the subject properties identified in the Official Community Plan is Single/Two Family Residential. The proposal is not consistent with this designation.

Rezoning to Higher Densities. Consider supporting an OCP amendment and rezoning application for residential densities greater than those provided for on the Generalized Future Land Use Map 19.1 in those cases where a portion of the proposed units are available for affordable, special needs or rental housing identified to be in short supply (guaranteed through a Housing Agreement), and where

:

- Supporting infrastructure is sufficient to accommodate the proposed development (or the developer is prepared to upgrade the necessary infrastructure); and

- *The proposed densities do not exceed the densities provided for on Map 19.1 by more than one increment (e.g. medium density multiple units might be entertained where low-density has been provided for, and low-density multiple units might be entertained where single/two unit residential densities have been provided for); and*
- *The project can be sensitively integrated into the surrounding neighbourhood, with no more than a one-storey height gain between the proposed development and the height permitted within land uses assigned to adjacent parcels (Where the property being proposed for redevelopment is large, consideration may be given to providing greater heights at the centre of the property provided that the new building is sensitively integrated with the surrounding neighbourhood); and*
- *Approval of the project will not destabilize the surrounding neighbourhood or threaten viability of existing neighbourhood facilities (e.g. schools, commercial operations etc.).*

The project meets most of the requirement outlined in the City of Kelowna's Official Community Plan. The OCP also contains the following objectives for context and design for new multiple family developments:

Objectives for Multiple Unit Residential Development

- All development should be an appropriate response to its physical context, or anticipated future context where an area is designated for increased density or land use transition in the OCP.
- All development within Urban Centres and Village Centres should contribute to the creation of pedestrian-oriented streets and public spaces.
- All development should contribute to a sense of community identity and sense of place (integration of development within larger community, belonging).
- All development should facilitate access by, and minimize conflicts among pedestrian, bicycle, and vehicular modes of transportation (access, mobility).
- All development should promote safety and security of persons and property within the urban environment (CPTED).

5.2 Kelowna Strategic Plan (2005)

The City of Kelowna Strategic Plan encourages the development of a more compact urban form by increasing densities through infill and redevelopment within existing urban areas and to provide for increased densities within future urban areas. Also redeveloping transitional areas to increase densities for more efficient use of existing land. Furthermore the City will emphasize a density of development higher than presently occurring to allow for more efficient use of the land.

6.0 TECHNICAL COMMENTS

6.1 Fire Department

Detailed drawings are required showing article 3.2.5.6, Access Route Design, of the BC Building Code is met. No parking signs are required on both sides of the street as per article 2.5.1.5 of the BC Fire Code. Fire department access, fire flows, and hydrants as

per the BC Building Code and City of Kelowna Subdivision Bylaw. Engineered fire flows are required and will help determine hydrant locations.

6.2 Inspections Services

Site plan does not appear to meet fire fighting requirements, turnarounds required on roads.

6.3 Works & Utilities

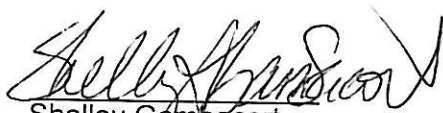
See Attached.

7.0 PLANNING AND DEVELOPMENT SERVICES DEPARTMENT

This proposal for Multiple Unit Residential – low density represents a one increment bump in density over the Single / Two Unit Residential designation. The OCP could support the provision of a one increment bump in density for townhouse development (Multiple Unit Residential – low density) typified by RM3 zoning if there were an affordable housing component that satisfies the conditions of OCP Policy 8.1.31. In order to support this increase it would require that 50% of the housing gained by the proposed rezoning and the OCP amendment should be reserved as affordable. As mentioned the applicant wishes to provide cash-in-lieu of entering into a Housing Agreement.

There had been neighbourhood concern that an environmentally sensitive wetland exists on the subject properties. However, the City's Environment Division has indicated that while a very small grassy swale is located on the subject properties it is not a wetland. As such, a Natural Environment Development Permit is not required. It should also be noted that Staff have received letters and a petition in opposition of the overall development proposal.

The proposed development is within walking distance of three schools and is located near the City's Dehart Transit Station. The quality of the development is high and gives the project a residential street appeal. The proposed landscaping will help to mitigate the developments impact on the adjoining areas.

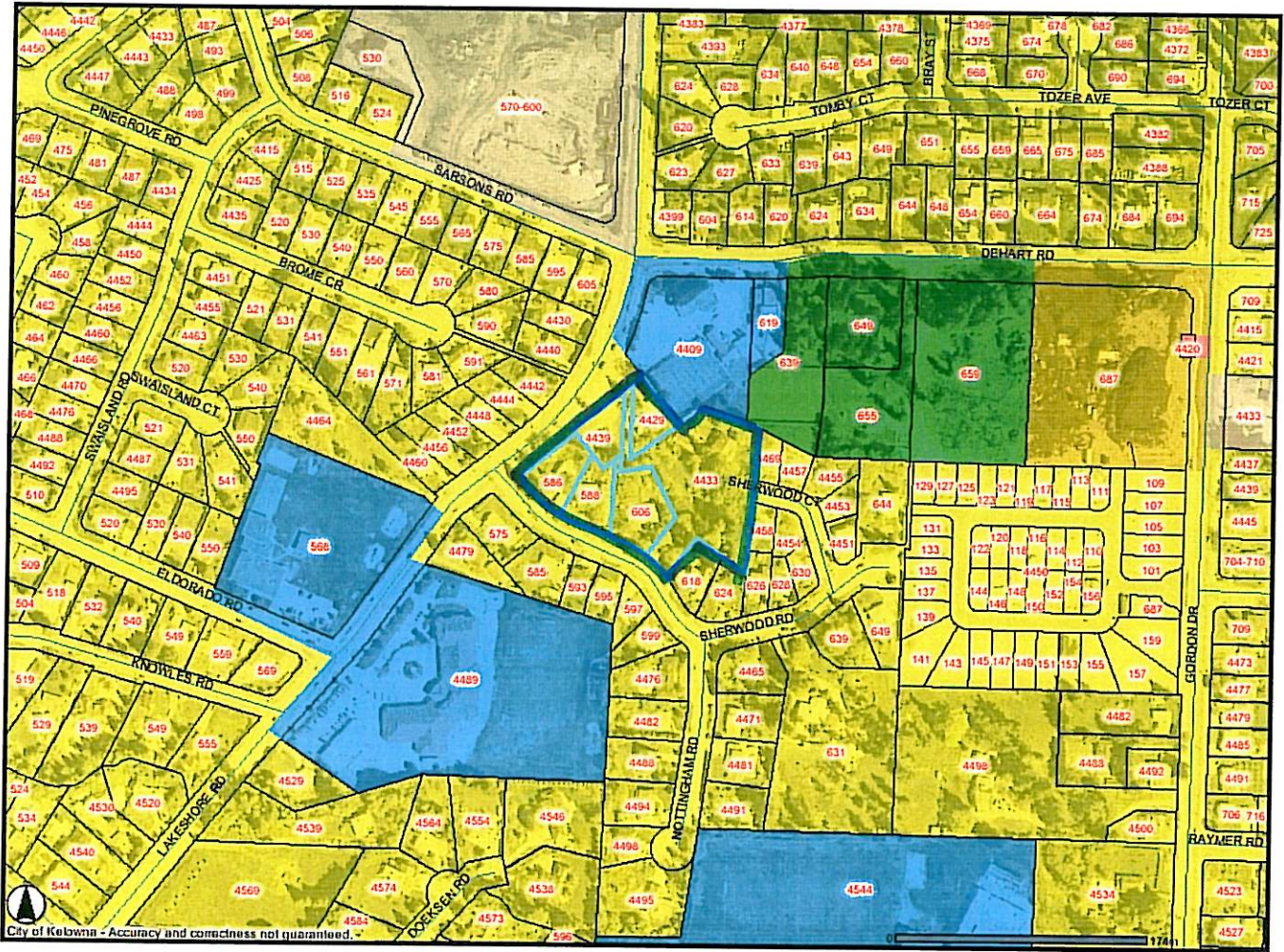


Shelley Gambacort
Current Planning Supervisor
SG/aw

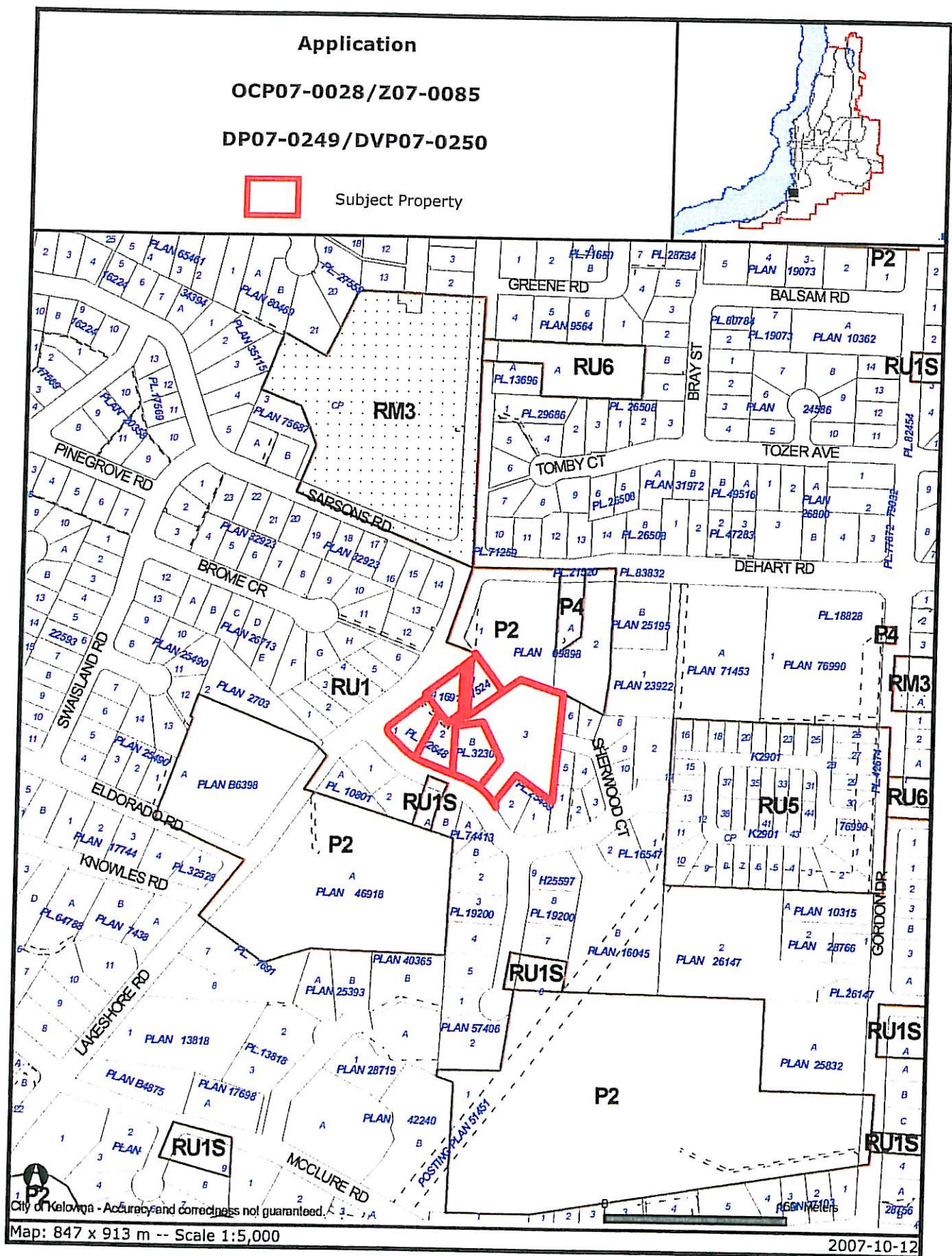
Attach.

- Location Map
- Future Land Use Map
- Site Plan & Elevations
- Variance Justification Letter
- Traffic Analysis
- Works & Utilities - Comments

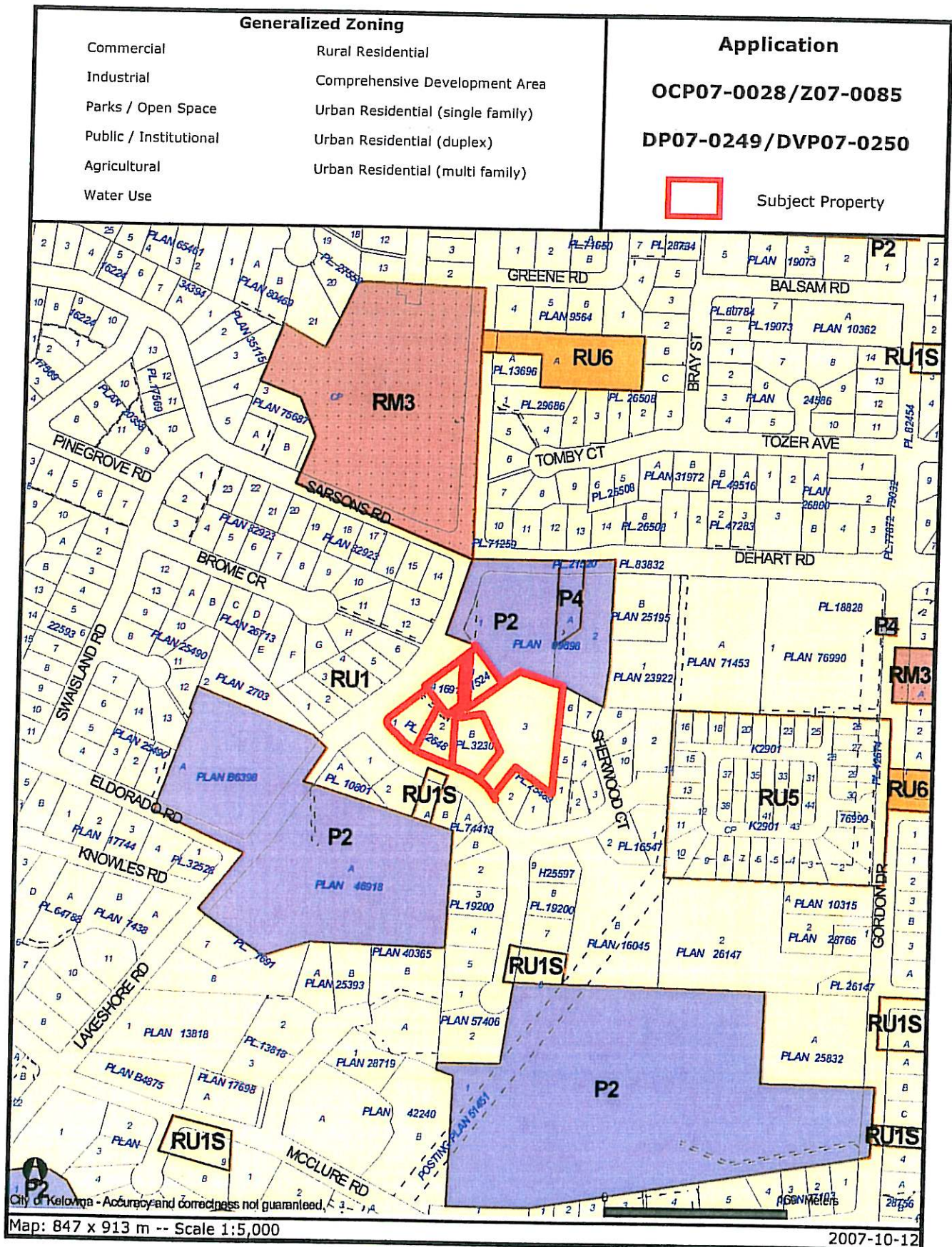
MAP "A"



Subject Property to have Future Land Use designation changed from "SINGLE/TWO UNIT RESIDENTIAL" to "MULTIPLE UNIT RESIDENTIAL – LOW DENSITY"



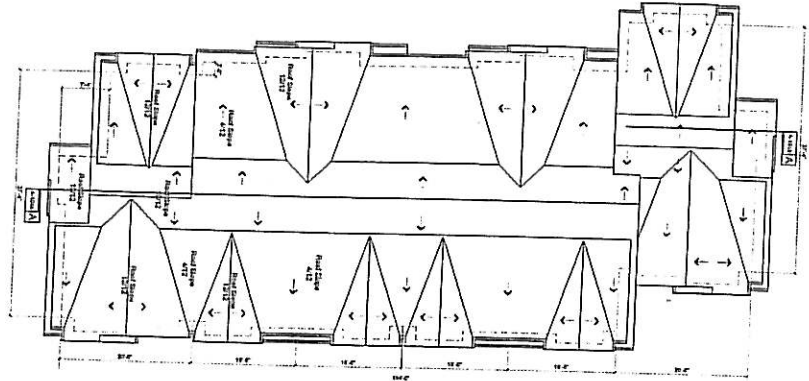
Certain layers such as lots, zoning and dp areas are updated bi-weekly. This map is for general information only.
 The City of Kelowna does not guarantee its accuracy. All information should be verified.



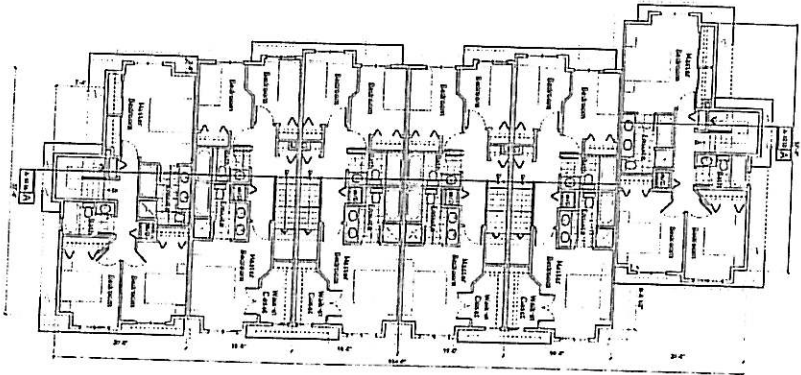
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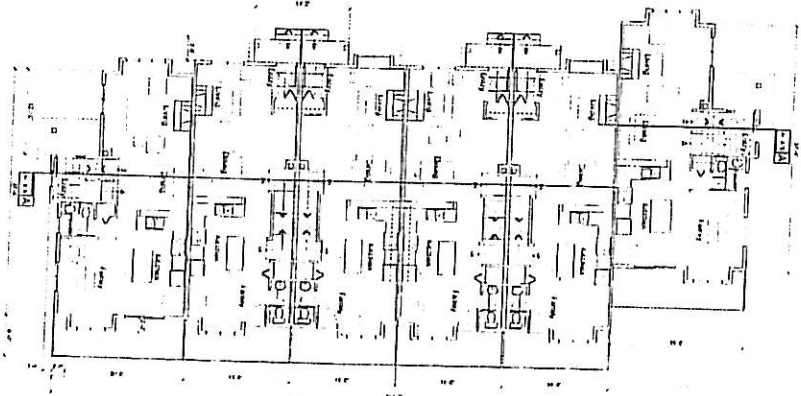
Roof Plan



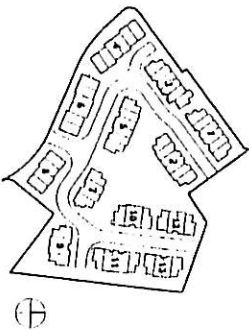
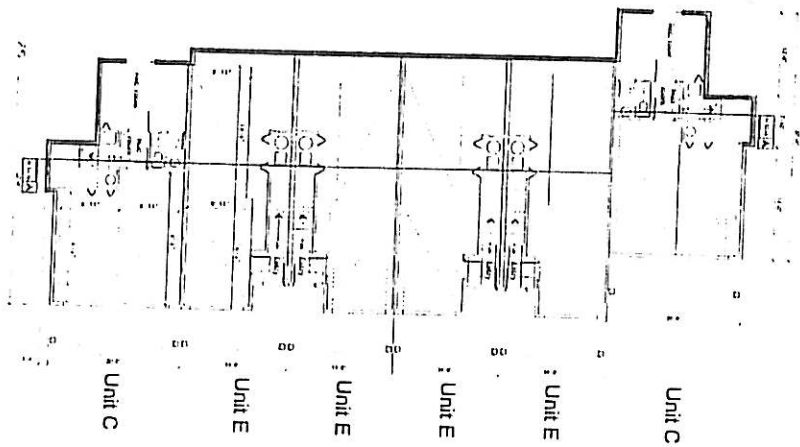
Second Floor Plan

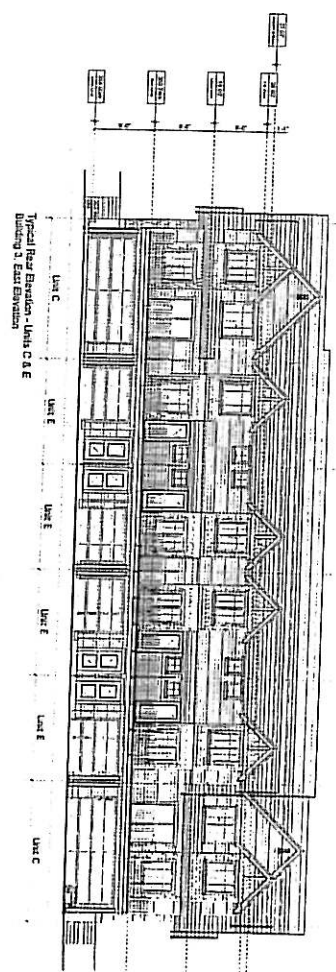


Main Floor Plan

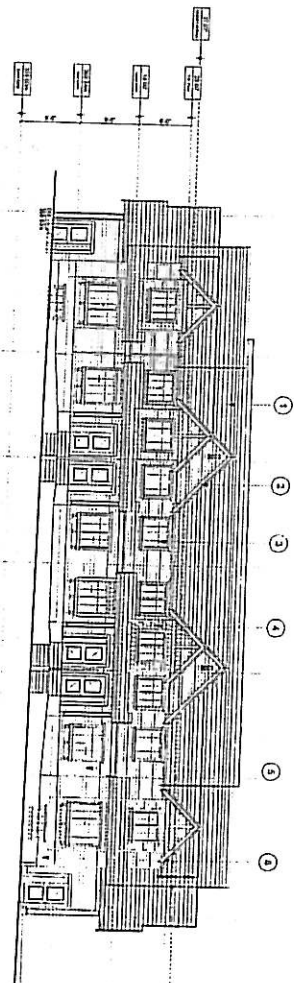


Lower Floor Plan

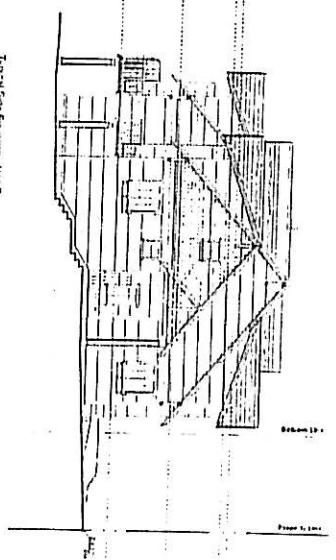




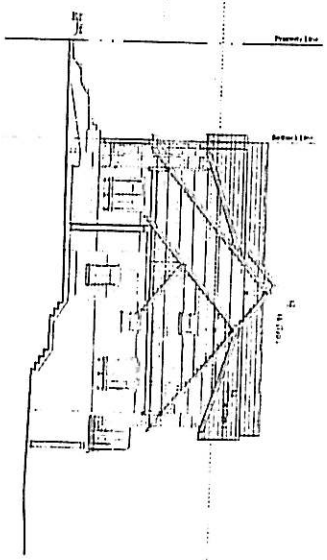
Typical Rear Elevation - Units C & E
Building 3, Rear Elevation



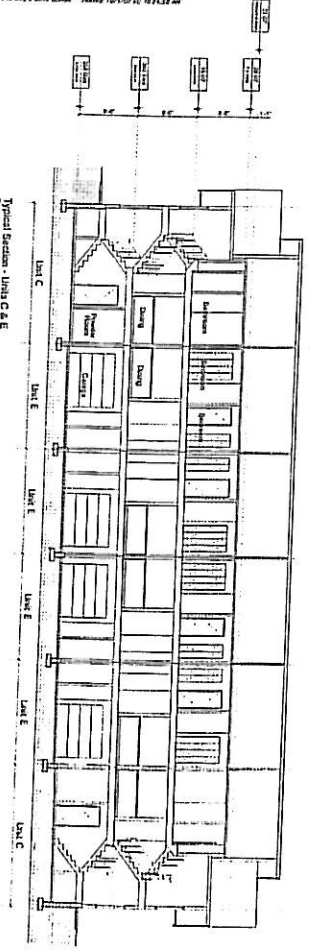
Typical Street Elevation - Units C & E
Building 3, West Elevation



Typical Side Elevation - Unit C
Building 3, North Elevation

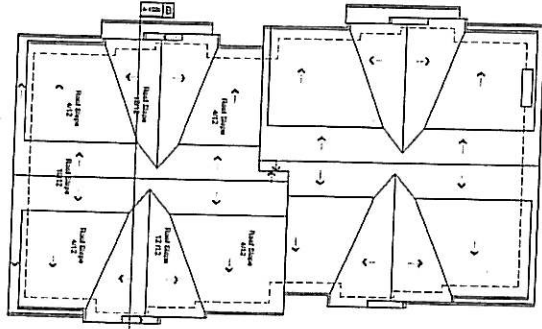


Typical Side Elevation - Unit C
Building 3, South Elevation

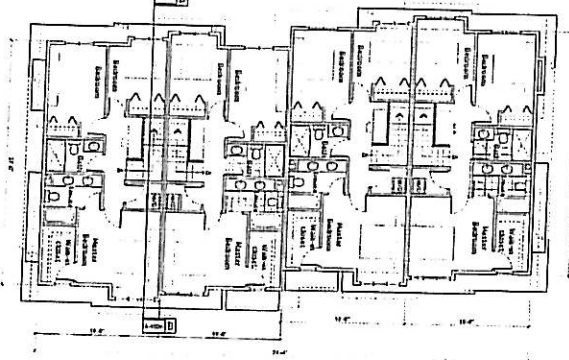


Typical Section - Units C & E
Building 3, Section AA

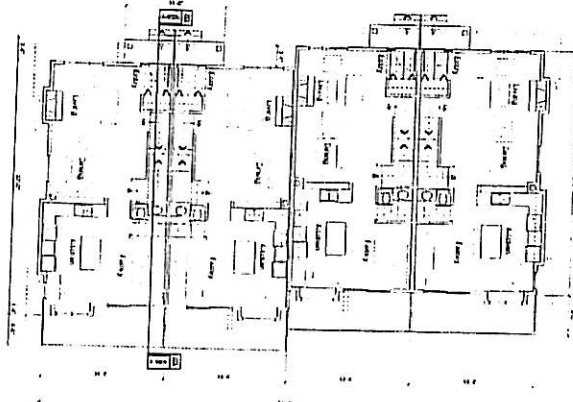
- MATERIALS**
- 1. Colonial Style Siding
 - 2. Colonial Style Shutters
 - 3. Colonial Style Windows
 - 4. Colonial Style Doors
 - 5. Colonial Style Roofs



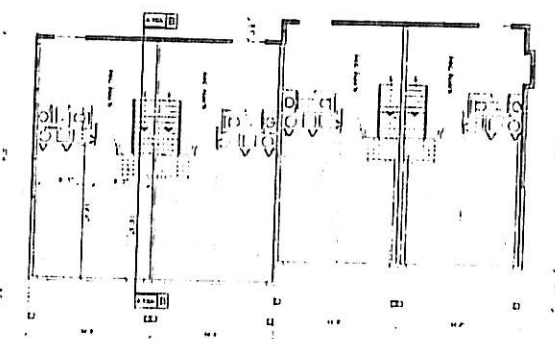
Roof Plan



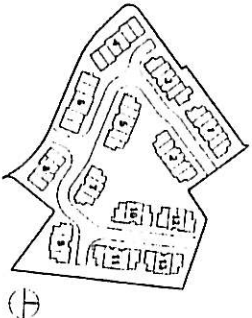
Second Floor Plan



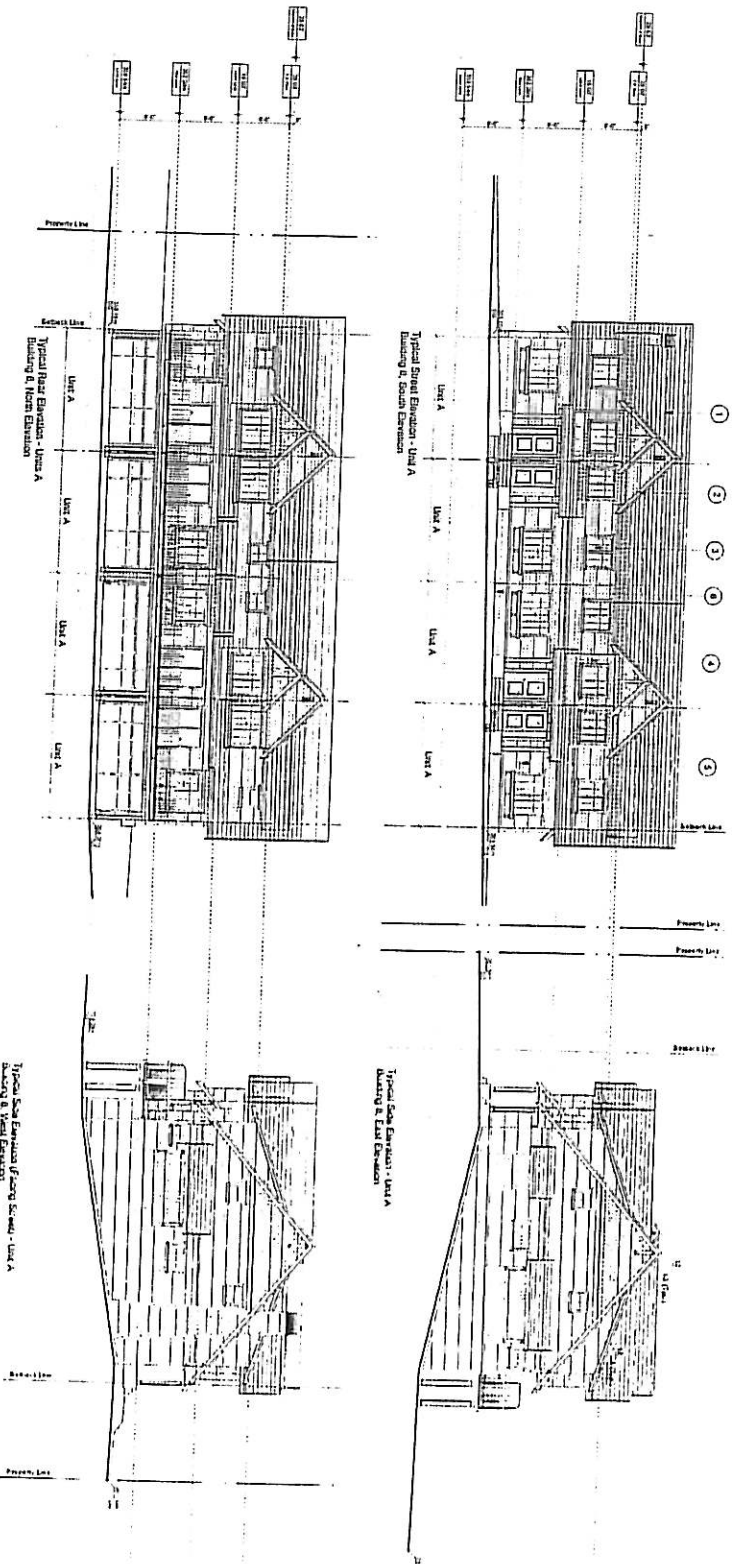
Main Floor Plan



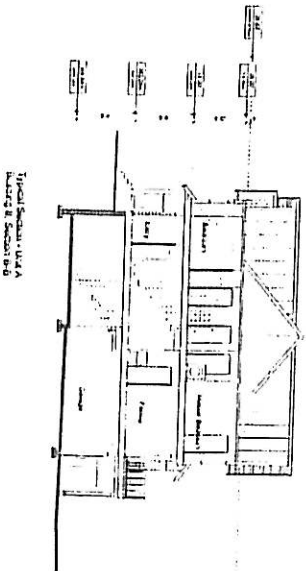
Lower Floor Plan



Architects
A-102a



- Materials**
- 1 Dark Stucco
 - 2 Wood Siding
 - 3 Vinyl Siding
 - 4 Hardwood Siding
 - 5 Hardwood Siding
 - 6 Cedar and Pine



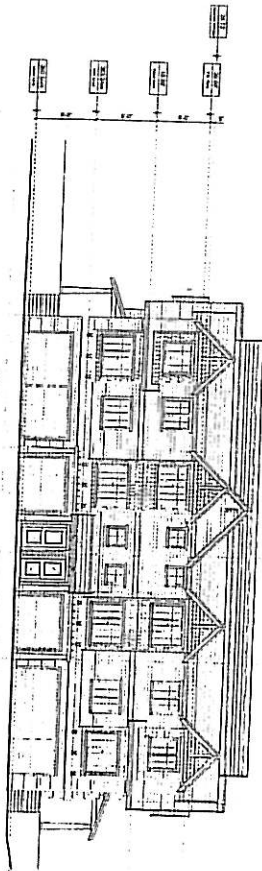
ARCHITECTS

Serial

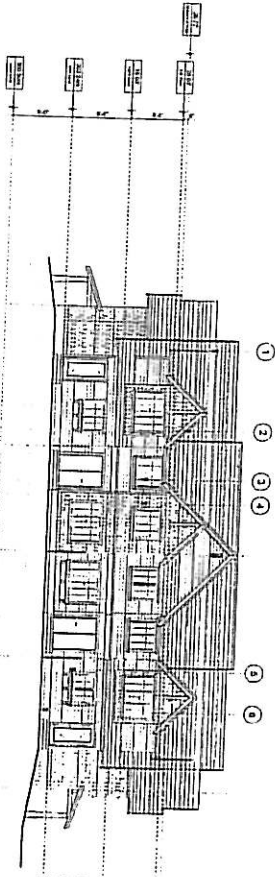
Building B
Elevations and Sections
(Units A-D)

A-102B

The floor plan of the second floor shows three distinct units. Unit B, on the left, contains a large living area, a kitchen, and a bathroom. Unit C, in the center, is a long, narrow unit with a central corridor and several rooms. Unit D, on the right, includes a living area, a kitchen, and a bathroom. A central staircase and common areas are located between the units. The plan is labeled with 'Unit B', 'Unit C', and 'Unit D' at the bottom.

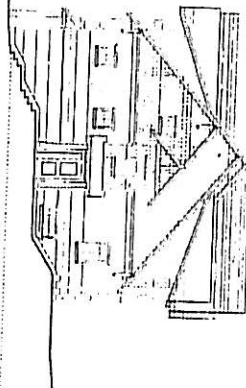


Typical Floor Section - Units B & D
Building 8, West Elevation

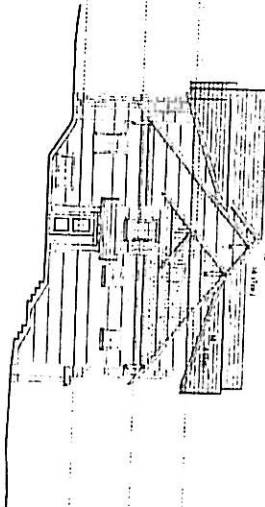


Typical Floor Section - Units B & D
Building 8, South Elevation

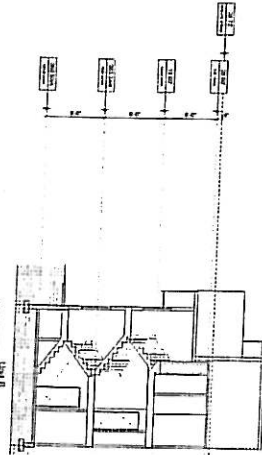
- Materials**
- 1. Paint
 - 2. Wood trim
 - 3. Vinyl flooring
 - 4. Ceramic tile
 - 5. Insulation
 - 6. Drywall
 - 7. Concrete
 - 8. Steel



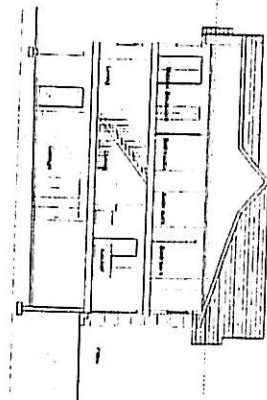
Typical End Section - Unit B
Building 8, West Elevation



Typical End Section - Unit B
Building 8, East Elevation



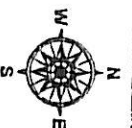
Typical Section - Unit B
Building 8, Section C



Typical Section - Unit D
Building 8, Section D

ARCHITECTS

Project: Building 8
Elevations and Sections
Units B & D
Date: 10/1/02
Scale: 1/8" = 1'-0"



**Sienna
in the
Mission**
Townhome Development
Kelowna, BC

Issue / Revision:	
1	Excluded in 1st
2	Revised for 2nd
3	
4	
5	

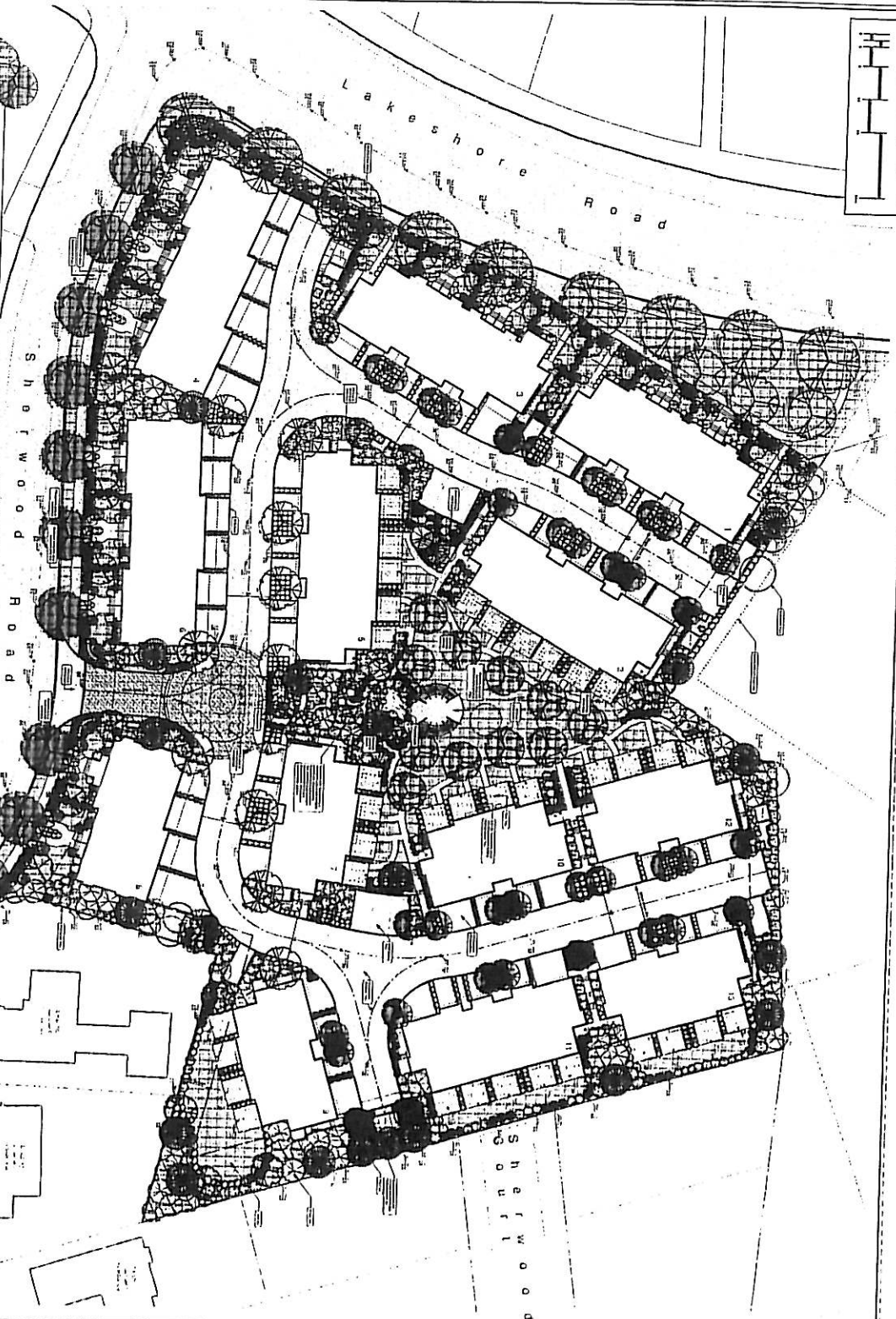
Printed by:	07-064
Design By:	FC
Drawn By:	SP
Checked By:	FC
Date:	Dec 1 2007
Scale:	1:200



Outland Design
Landscape Architecture
205-1889 Spall Road
Kelowna BC V1Y 4R2
Tel: (250) 868-9270
Fax: (250) 868-9279
www.outlanddesign.ca

L.I.O.F.I.

Conceptual Landscape Plan

[illegible][illegible]

October 2, 2007

City of Kelowna
Planning & Development Services
1435 Water St.
Kelowna, BC V1Y 1J4

Attention: Members of City Council

NSDA
ARCHITECTS

201-134 Abbott St
Vancouver BC
Canada V6B 2K4

T 604.669.1926
F 604.683.2241

info@nsda.bc.ca
www.nsda.bc.ca

A Corporate Partnership

Dear Council Members,

Re: Sienna Townhouses

Applicant - Allaire Properties (Lakeshore Inc.)
Location - Lakeshore Road & Sherwood Road

On behalf of our client, Allaire Properties, we would like to request variances for the following bylaws in the above-mentioned project:

1. Zoning Bylaw 13.9.7(c) – Private open space provided per dwelling, and
2. Zoning Bylaw 6.4.1 - Projection into yards

1. Zoning Bylaw 13.9.7(c) – Private open space per dwelling

We would like to ask for a relaxation of the private open space required for the street-facing units in Buildings 1, 3, 4, 6, and 8. Doing so would allow the design to incorporate a large community garden area, which would be a unique and valuable outdoor amenity for these residents. Since it is central and easily accessible to all units within the development, residents of these particular buildings can take advantage of this outdoor space in addition to their large, full-width, 6-ft deep deck that faces this interior garden. This would be particularly beneficial to families with children or pets, who would need larger spaces for recreation and play.

2. Zoning Bylaw 6.4.1 - Projection into yards

We have proposed to have the entry stairs project into the front yard up to one meter maximum for Buildings 1, 3, 4, 6, and 8. Reasons for this design decision are two-fold:

- a) In order to accommodate the above-mentioned outdoor community garden, buildings on the site must be shifted slightly outward toward the property lines.
- b) The street-facing units are south and west of Buildings 2, 5, and 7. By locating them as shown, it gives more space between the buildings on either side of the interior road and, consequently, allows more daylight to reach these inner units.

Sincerely,



Tom Staniszakis, MAIBC
Neale Staniszakis Doll Adams Architects

October 29, 2007
File: 07072-5 001

Tel: (604) 807-9887

Allaire Properties Inc.
4025 McConnell Dr
Burnaby, BC, V5A 3A7

Attention: Mr. Marc Allaire

Dear Sir:

**Reference: Mission Townhouse Project, Kelowna BC
Traffic Review**

We are pleased to provide the following review of the anticipated traffic generated by the proposed 65 unit Townhouse site on Sherwood Road.

Traffic impact reviews are based on trip generation rates. The rates are based on information collated from actual traffic studies, and presented for the average weekday Peak Hour volumes the specific land use will generate, during normal operations. The trip generation rates applied for typical residential developments are from the Ministry of Transportation Trip Generation Rates Manual Table 7-1, as follows:

- Single-family Homes, with an AM Peak Hour (between 8:00 AM and 9:00 AM) rate of 1.0 trip per unit and PM Peak Hour (between 4:15 PM and 5:15 PM) rate of 1.2 trips per unit;
- Town Homes, with an AM rate of 0.49 trips per unit and PM rate of 0.68 trips per unit.

As shown on the following figure, Sherwood Road currently has 36 single family home sites with access on to Sherwood Road. Based on the above single family rates, Sherwood Road is currently generating 36 combined inbound and outbound trips during the AM peak hour and 43 trips during the PM peak hour.

Under the current Official Community Plan future land use designation for the area, single family and two unit residential (duplex) is supported for the site. The 6 existing lots, from the proposed site, could be reconfigured to develop 14 duplex homes all with access onto Sherwood Road, for a total of 28 new units added to the Sherwood Road neighbourhood. This would bring the total of homes on Sherwood Road up to 60, and result in 60 AM peak hour trips, and 72 PM peak hour trips.

October 29, 2007
Mr. Marc Allaire
Allaire Properties Inc.
Page 2 of 2

**Reference: Mission Townhouse Project, Kelowna BC
Traffic Review**

The proposed site would consolidate the 6 single family lots into one strata plan, for the development of 65 townhouse units. The access to the site would be off of Sherwood Road. Lakeshore Road is designated as an arterial road, with access restricted to collector roadways.

Based on the above, the Sherwood Road Town House Site is anticipated to generate an average of 32 vehicle trips during the AM Peak Hour and 44 vehicle trips during the PM Peak Hour.



Existing Neighbourhood

Page 3 of 3

Reference: Mission Townhouse Project, Kelowna BC
Traffic Review

With the development of the Town House site, combined with the existing 32 single family homes in the Sherwood neighbourhood, the total traffic on the western end of Sherwood Road would be 64 trips during the AM peak hour, and 82 trips during the PM peak hour.

This works out to an increase of 4 trips during the AM peak hour, and 10 trips during PM peak hour, when compared to the Official Community Plan land use build out of the site combined with the existing Sherwood Road neighbourhood and an increase of 28 trips during the AM peak hour, and 39 trips during PM peak hour, when compared to the existing Sherwood Road properties and neighbourhood.

This works out to an average duration of three minutes between new vehicle trips on the existing portion of Sherwood Road, between Lakeshore Road and the access to the site, during the peak afternoon travel period.

Based on the above review, the traffic generated by the proposed townhouse development will have a minimal impact on the operation of the adjacent street network, and is a consistent use of the street classifications as presented in the City of Kelowna Official Community Plan.

We trust the above meets your requirements. Please contact the undersigned if you have any questions on the above or require further information.

Sincerely,

CTQ CONSULTANTS LTD.

Per:

[Signature]

Mr. David D. Cullen, P.Eng.

Transportation Engineer

DDC:

Enclosures

CITY OF KELOWNA
MEMORANDUM

Date: December 20, 2007
File No.: Z07-0085 DP07-0249 DVP07-0250 OCP07-0028
To: Planning & Development Services (AW)
From: Development Engineering Manager (SM)
Subject: Sherwood Rd, Lakeshore Rd 64 unit townhouses Sienna

The Works & Utilities Department have the following comments and requirements associated with this application to rezone from RU-1 to RM3. The road and utility upgrading requirements outlined in this report will be a requirement of this development.

The Development Engineering Technologist for this project is John Filipenko. AScT

1. Domestic Water and Fire Protection

- (a) The existing lots are serviced with small diameter water services. The developer's consulting mechanical engineer will determine the domestic and fire protection requirements of this proposed development and establish hydrant requirements and service needs.
- (b) Only one service will be permitted for this development. The applicant, at his cost, will arrange for the removal of all existing services and the installation of fire hydrants and one new larger metered water service. The estimated cost of this construction for bonding purposes is **\$24,000.00**
- (c) A water meter is mandatory for this development and must be installed inside the building on the water service inlet as required by the City Plumbing Regulation and Water Regulation bylaws. The developer or building contractor must purchase the meter from the City at the time of application for a building permit from the Inspection Services Department, and prepare the meter setter at his cost. Boulevard landscaping, complete with underground irrigation system, must be integrated with the on-site irrigation system.

2. Sanitary Sewer

- (a) The developer's consulting mechanical engineer will determine the requirements of this proposed development and establish the required size and preferred location of the new service. Only one service will be permitted for this development. The applicant, at his cost, will arrange for the removal of all existing small diameter services and the installation of a new larger service. The estimated cost of this construction for bonding purposes is **\$20,000.00**
- (b) The property is located in Sewer Specified Area No 17, and therefore will be subject to sewer Specified Area charges that must be paid in full. The cash commute amount (valid until March 31, 2008) is \$730.73 per SFE.

3. Storm Drainage

- (a) The developer must engage a consulting civil engineer to provide a storm water management plan for each site, which meets the requirements of the City Storm Water Management Policy and Design Manual. The storm water management plan must also include provision of a lot grading plan, identify minimum basement elevation (MBE), overland drainage routes, floodplain elevations and setbacks, and provision of a storm drainage service for the lot and /or recommendations for onsite drainage containment and disposal systems. The on-site drainage systems may be connected to the street drainage system with overflow services. The estimated cost of this construction for bonding purposes is **\$10,000.00**
- (b) The subject development must include the design of drainage system for Lakeshore Road and Sherwood Road fronting this development, including manholes and catch basins for curb and gutter construction. The cost of the work was included in the road design calculations.
- (c) Storm drainage systems and overflow service(s) for the site will be reviewed and approved by Engineering when site servicing designs are submitted.

4. Road Improvements

- (a) Lakeshore Road must be upgraded to a full urban standard along the full frontage of this proposed development, including curb and gutter, separate sidewalk, storm drainage system including catch basins, manholes / dry-wells, pavement widening and line painting. Also required is a landscaped boulevard complete with underground irrigation system, street lighting and re-location or adjustment of utility appurtenances if required to accommodate the upgrading construction. The estimated cost for this construction for bonding purposes is **\$115,000.00**. Not included in the estimate are costs for pole relocations.
- (b) Sherwood Road must be upgraded to a full urban standard (SS-R7) along the full frontage of this proposed development, including a barrier curb and gutter, monolithic sidewalk, storm drainage system including catch basins, manholes / dry-wells and pavement widening. Service upgrades will require additional road cuts. The developer will be required to provide a pavement overlay to the road centerline. Also required is a landscaped boulevard complete with underground irrigation system, street lighting and re-location or adjustment of utility appurtenances if required to accommodate the upgrading construction. The estimated cost for this construction for bonding purposes is **\$67,000.00**.
- (c) Provide Street Sign and Markings Drawings. The City will install or modify the required signs and traffic control devices at the developer's expense.

5. Road Dedication and Subdivision Requirements

By registered plan to provide the following:

- (a) Grant Statutory Rights Of Way if required for utility services.
- (b) Provide an additional highway allowance widening along the frontage of Lakeshore road to provide a total right of way width of 30.0m.
- (c) Dedicate a 6.00m radius corner rounding of the Lakeshore Rd and Sherwood Rd intersection.
- (d) Excess road right of way on Lakeshore Rd frontage may be available to the developer. Contact The City Community Development & Real Estate Department

- (e) Lot consolidation.
- (f) If any road dedication affects lands encumbered by a Utility right-of-way (such as Gas, etc.) please obtain the approval of the utility. Any works required by the utility as a consequence of the road dedication must be incorporated in the construction drawings submitted to the City's Development Manager.

6. Electric Power and Telecommunication Services

The existing overhead electrical and telecommunication distribution wiring on Lakeshore Road fronting this development must be relocated to an approved offset within the proposed boulevard area. The developer may choose to have the wiring installed in an underground duct system. The buildings must be connected by underground services.

It is the developer's responsibility to make a servicing application with the respective electric power, telephone and cable transmission companies to arrange for these services, which would be at the applicant's cost. If the electrical source is on the west side of Lakeshore Road, this source must be in underground ducts.

7. Street Lighting

Street lighting including underground ducts must be installed on both Sherwood Road and Lakeshore Road fronting on the proposed development. The cost of this requirement is included in the roads upgrading item.

8. Engineering

Road and utility construction design, construction supervision, and quality control supervision of all off-site and site services including on-site drainage collection and disposal systems, must be performed by an approved consulting civil engineer. Designs must be submitted to the City Engineering Department for review and marked "issued for construction" by the City Engineer before construction may begin.

9. Geotechnical Report

As a requirement of this application and building permit approval the applicant must provide a comprehensive geotechnical report prepared by a Professional Engineer qualified in the field of hydro-geotechnical survey to address the following:

- (a) Area ground water characteristics, including water sources on the site.
- (b) Site suitability for development; i.e. unstable soils, foundation requirements etc.
- (c) Drill and/or excavate test holes on the site and install piezometers if necessary. Log test hole data to identify soil characteristics, identify areas of fill if any. Identify unacceptable fill material, analyse soil sulphate content, identify unsuitable underlying soils such as peat, etc. and make recommendations for remediation if necessary.
- (d) List extraordinary requirements that may be required to accommodate construction of roads and underground utilities as well as building foundation designs.
- (e) Additional geotechnical survey may be necessary for building foundations, etc.

10. Survey Monuments and Iron Pins

If any legal survey monuments or property iron pins are removed or disturbed during construction, the developer will be invoiced a flat sum of \$1,200.00 per incident to cover the cost of replacement and legal registration. Security bonding will not be released until restitution is made.

11. Development Permit and Site Related Issues

- (a) The request to vary private open space requirements and projections into yards, do not compromise Works and Utilities servicing requirements.
- (b) A site grading plan, a site servicing plan and a storm drainage plan are a requirement of this application. Site servicing issues and road access will be further reviewed and comments related to site development will be addressed when a detailed site development design or building permit application is received.
- (c) Access and Manoeuvrability
 - (i) Direct access onto Lakeshore Road will not be permitted. The proposed access location onto Sherwood Road is acceptable as shown.
 - (ii) An SU-9 standard size vehicle must be able to manoeuvre onto and off the site without requiring a reverse movement onto public roadways. If the development plan intends to accommodate larger vehicles movements should also be illustrated on the site plan.

12. Bonding and Levy Summary

(a) Bonding

Service Upgrades	\$ 54,000.00
Lakeshore Road Frontage Improvements	\$115,000.00
Sherwood Road Frontage Improvements	\$ 67,000.00

Total **\$ 236,000.00**

NOTE: The bonding amount shown above are comprised of estimated construction costs escalated by 140% to include engineering design and contingency protection and are provided for information purposes only. The owner should engage a consulting civil engineer to provide detailed designs and obtain actual tendered construction costs if he wishes to do so. Bonding for required off-site construction must be provided and may be in the form of cash or an irrevocable letter of credit, in an approved format.

The owner must also enter into a servicing agreement in a form provided by the City.

Sanitary Sewer Specified Area 17 charge.

The cash commute amount is \$730.73 per SFE.

Based on the proposed unit count of 64, the amount would be \$ 46,766.72

Total **\$ 46,766.72**

13. OCP Amendment

We have no concerns or requirements of the OCP amendment application.

14. Administration Charge

An administration charge will be assessed for processing of this application, review and approval of engineering designs and construction inspection. The administration charge is calculated as (3% of Total Off-Site Construction Cost plus GST) in the amount of **\$5,406.00** (\$5,100.00 + 306.00 GST)

Steve Muenz, P. Eng.
Development Engineering Manager
JF/jf